



ARCHAEOLOGICAL SERVICES

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May 20, 2018

Nicole White
PND Engineers, Inc.
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MEMORANDUM

RE: Updated plans for the removal of piles at the Port of Kalama T-Barge Facility project area, Cowlitz County, Washington

Nicole,

This memorandum addresses the cultural resource implications of the proposed removal of piles from the Port of Kalama T-Barge Facility project area. The project concerns the placement of a floating T-barge along with access and utility infrastructure in NE $\frac{1}{4}$ of Section 18, Township 6 North, Range 1 West, Willamette Meridian, on the Columbia River shoreline within the city of Kalama, Cowlitz County, Washington (Figures 1 and 2). When the project was first brought to the attention of Archaeological Services, LLC (ASCC) in 2017, the proposed work entailed the removal of an estimated 80 derelict, creosote-treated timber piles, all over 50 years of age (Figure 3). The number of piles slated for removal was reduced to 10, but out of concerns for navigation safety, plans were revised again and now call for removing a total of 61 historic timber piles and 2 modern steel piles (Figures 4-7).

As stated in ASCC's *Cultural Resource Literature Review of the Port of Kalama T-Barge Facility Project Area, Port Of Kalama, Cowlitz County, Washington* (Smith and Gall 2017) ASCC found no definitive information about the age or construction of the 61 historical piles during background research. However, some functional assumptions can be made based on the mapped positions of the piles. The clustered piles in the south of the pile removal area were likely dolphins for mooring vessels, log rafts, or both, and may have also functioned as corner supports on a larger dock structure. The lines of piles paralleling the shoreline likely represent one or more former docks, although no such structures are visible in this location on the reviewed historic maps (1863-present) or aerial photos (1951-present).

The only known structure identified by background research near the project area is the former dock/building of the Doty Fish Company, which stood from ca. 1895 to the 1970s to the immediate south of the pile removal area. The fish company

building is shown on the 1951 aerial in Figure 8, overlaid with the current project area. The accuracy of this overlay is imperfect, due to the geometric distortion of the aerial, but it does appear that the southernmost cluster of derelict piles could be the remains of a dock-corner dolphin similar to that shown in Figure 9. The dolphin in Figure 9 once stood at the southwestern corner of the Doty Fish Company dock and apparently served both mooring and structural purposes, suggesting that there may have been a counterpart at the dock's northwestern corner.

The historical associations of all piles proposed for removal—including those piles nearest the former Doty Fish Company dock—remain speculative. Over the long historic use of the Kalama riverfront, destructive flooding has prompted the rebuilding of docks multiple times (Hinchliff 2011). What remains in the water is likely a palimpsest of constructions separated by many years, ending with the ca. 1990 installation of the two steel piles amid the timbers. Background research does not suggest that the remaining timber piles represent significant historic data.

If you have any questions regarding ASCC's research, please don't hesitate to contact our office.

Sincerely,

Michael Smith, B.A. and Alexander Gall, M.A. RPA
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References

Hinchliff, Catherine

2011 A Group of Kalama Residents Meets to Organize Port of Kalama on December 22, 1919. *HistoryLink.org* Essay 9736, accessed online at <http://www.historylink.org/File/9736>

Smith, Michael and Alexander Gall

2017 *Cultural Resource Literature Review of the Port of Kalama T-Barge Facility Project Area, Port Of Kalama, Cowlitz County, Washington*. ASCC Report No. 17859. On file at the Washington State Department of Archaeology and Historic Preservation, Olympia, WA.

Thomas, C. Louise

2016 *Kalama: Images of America*. Arcadia Publishing, Charleston, South Carolina.

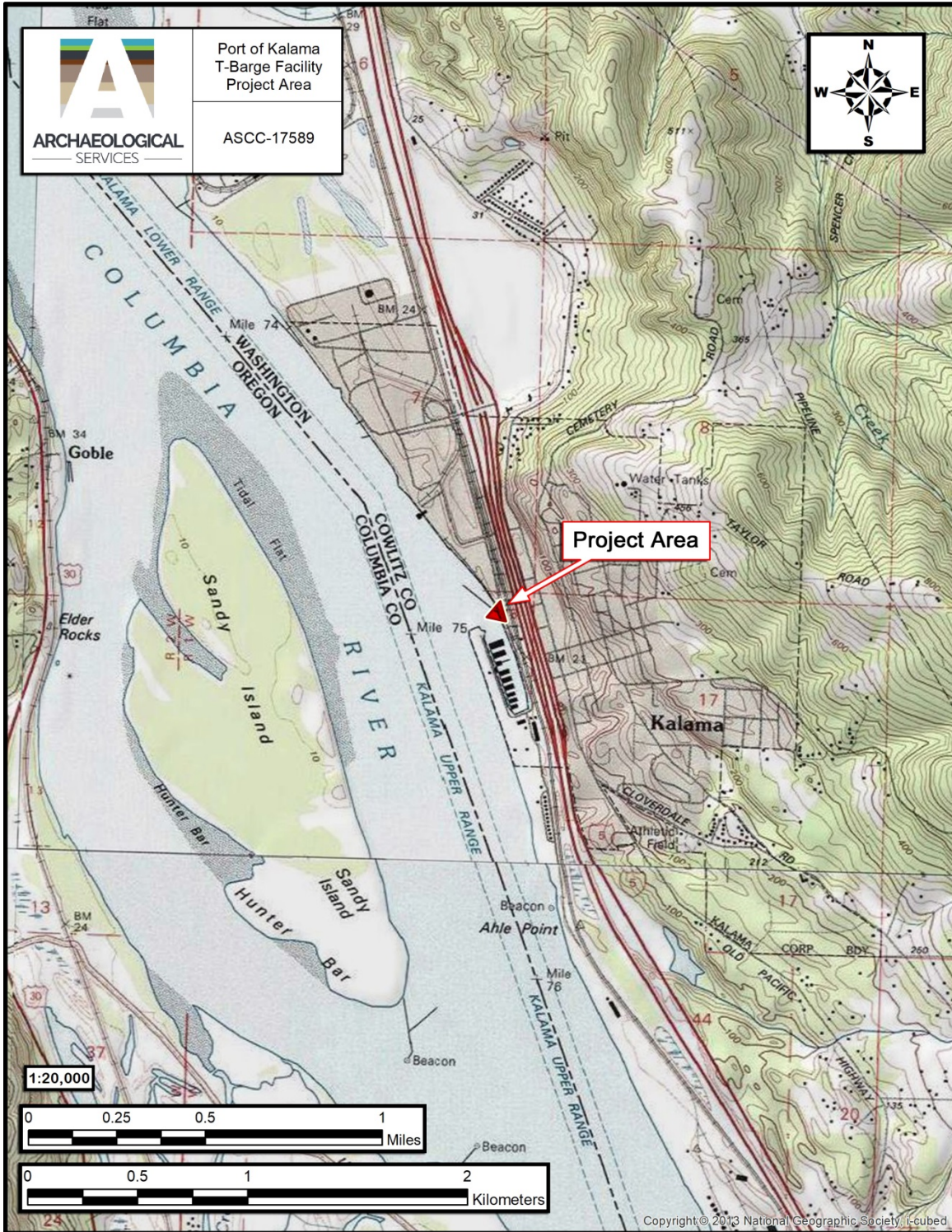


Figure 1. 1990 USGS Kalama, WA quadrangle, 7.5-minute series, showing the location of the project area in Sections 7 and 18, Township 6 North, Range 1 West.

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Figure 2. Aerial photomap showing an overview of the project setting and the configuration of detail maps in Figure 3 and Figure 4.

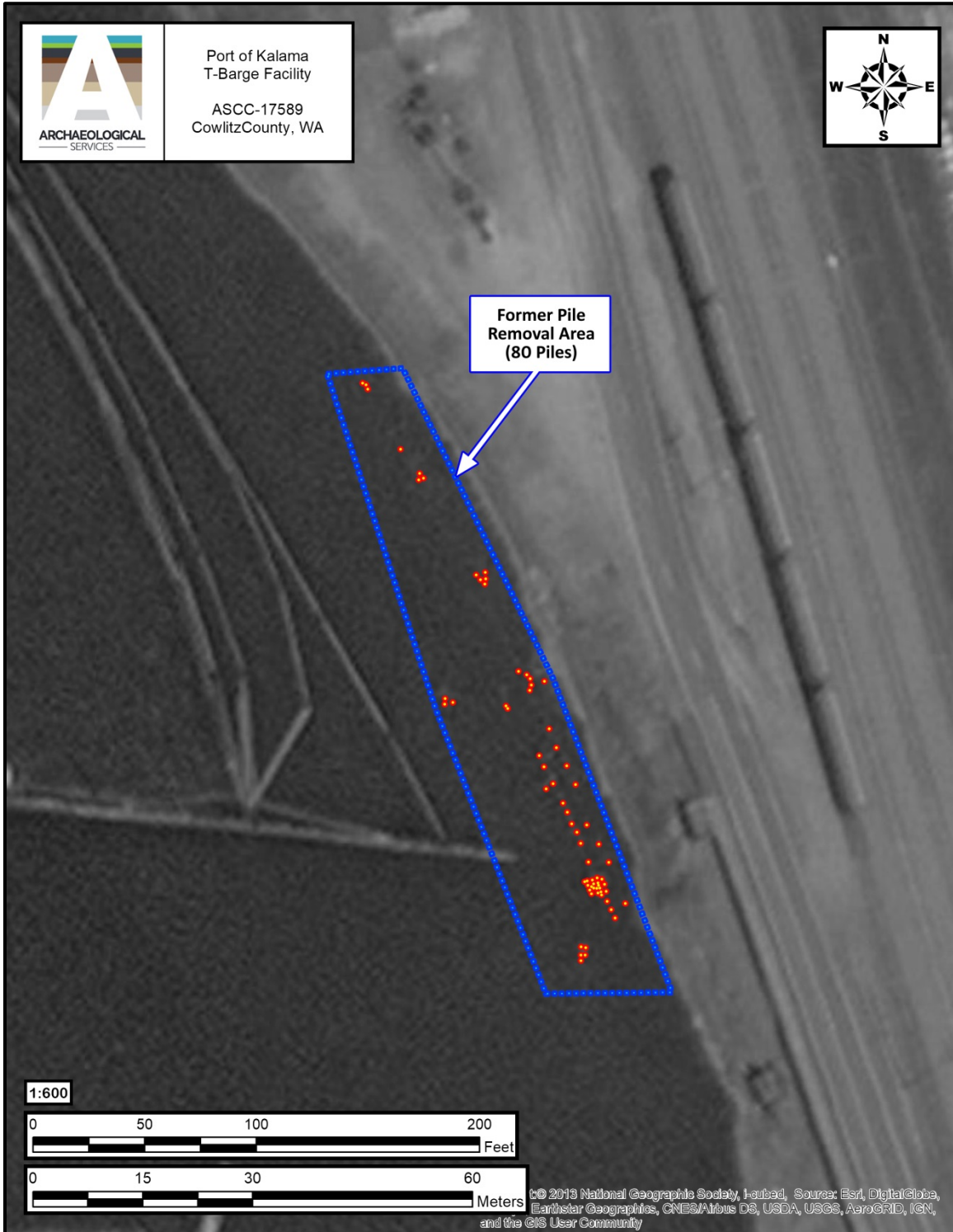


Figure 3. Aerial photomap showing the locations of piles previously slated for removal (including 80 historic timber piles) under the former project design.



Figure 4. Aerial photomap showing the piles currently slated for removal (including 61 historic timber piles) per the updated project design.

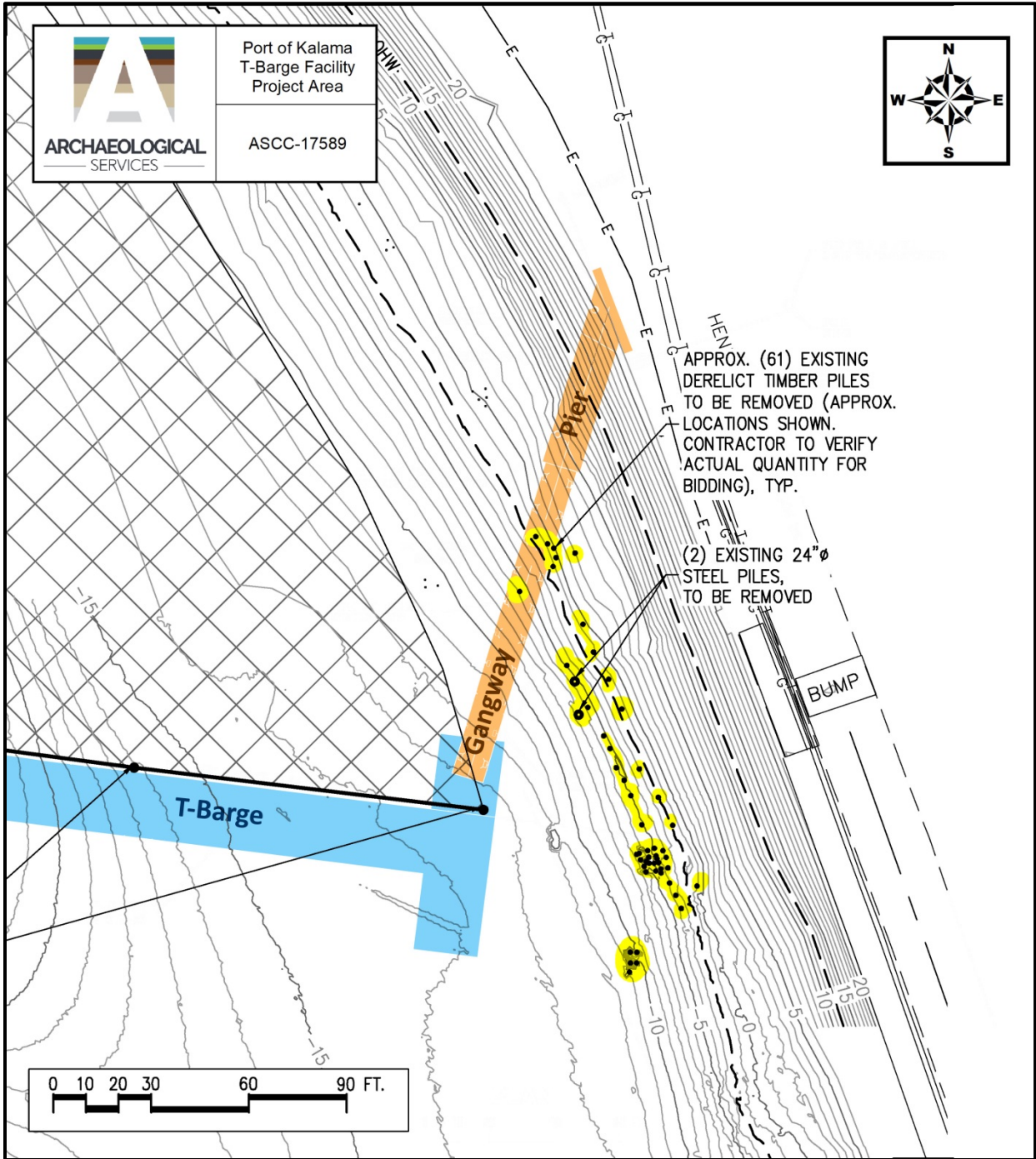


Figure 5. Updated plans for project area showing the timber and steel piles currently proposed for removal (highlighted in yellow).



Figure 6. Northwest-facing view of the northern piles proposed for removal, with the two steel piles at photo center.



Figure 7. Southwest-facing view of the southernmost piles proposed for removal.

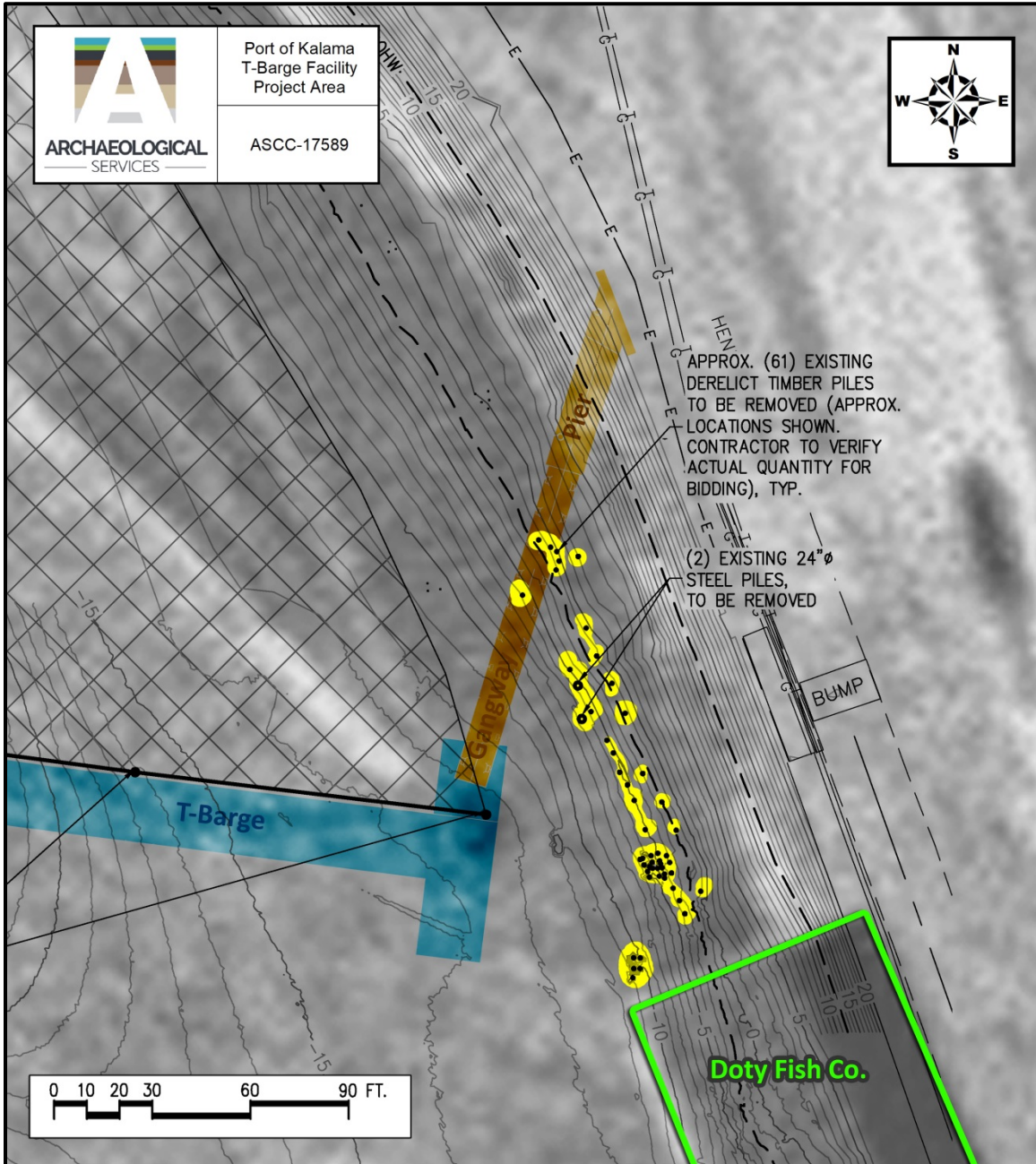


Figure 8. Detail of 1951 aerial photomap of the project area overlaid with the updated pile removal plans. The Doty Fish Company is building outlined in green.



Figure 9. Undated, northeast-facing photograph of the rebuilt Doty Fish Company building/dock structure, taken sometime after the original building was destroyed by fire in 1924 (Thomas 2016:48). The currently proposed pile removal area is located to the immediate north of this location (photo left).